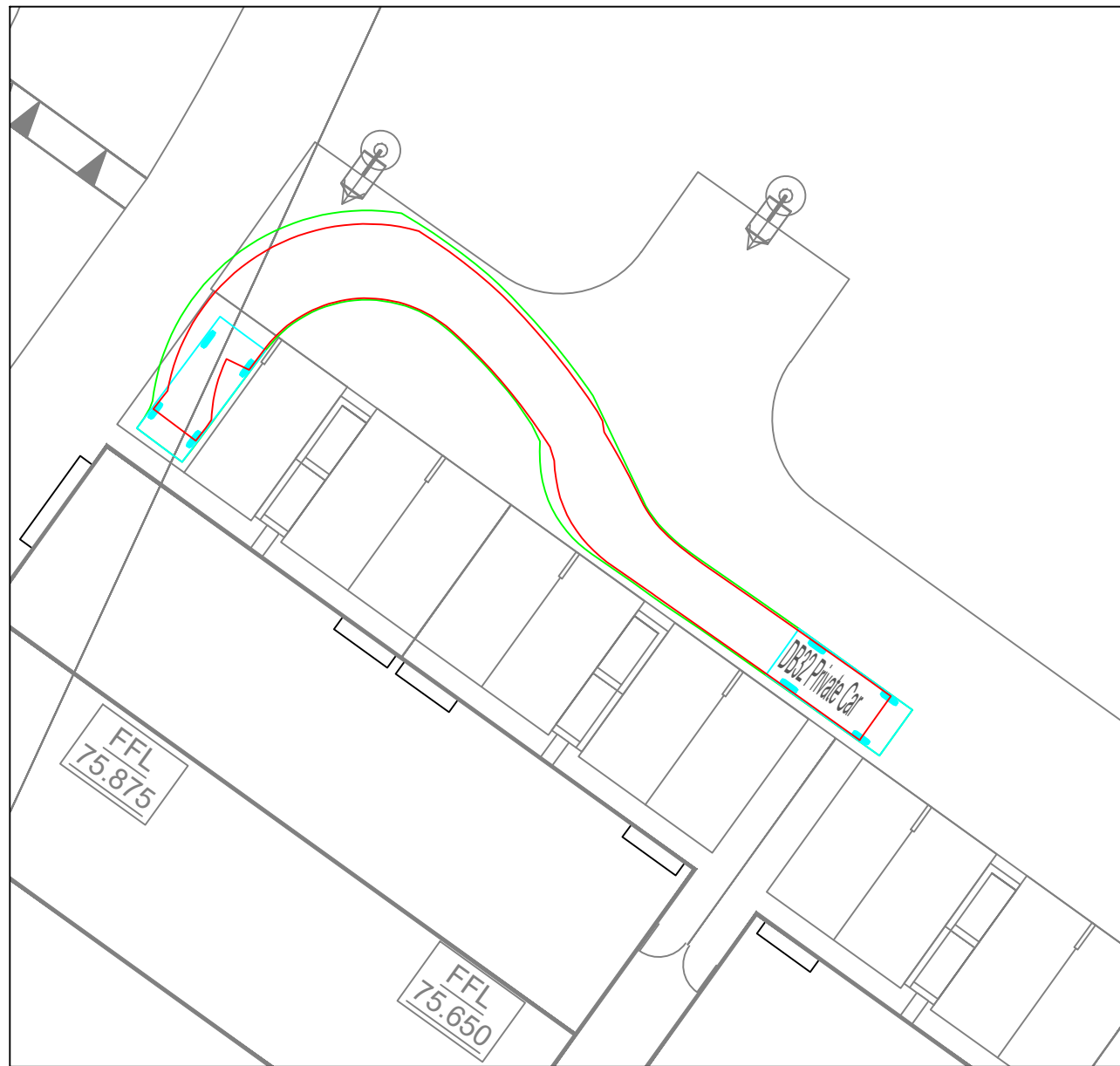




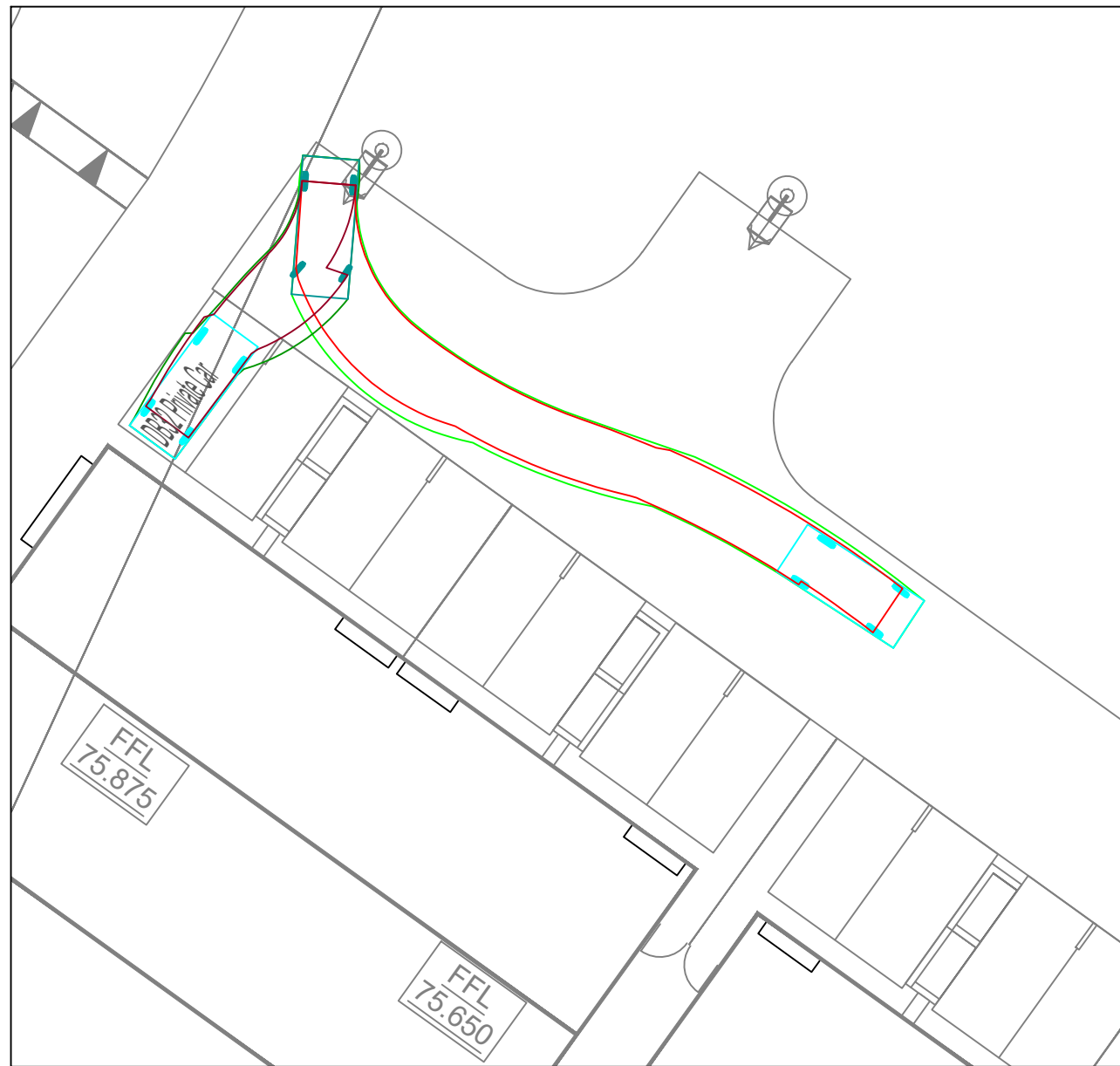
AUTOTRACK STANDARD CAR - FORWARD PARKING AREA 3
SCALE: 1:200



AUTOTRACK STANDARD CAR - REVERSING OUT AREA 3
SCALE; 1:200



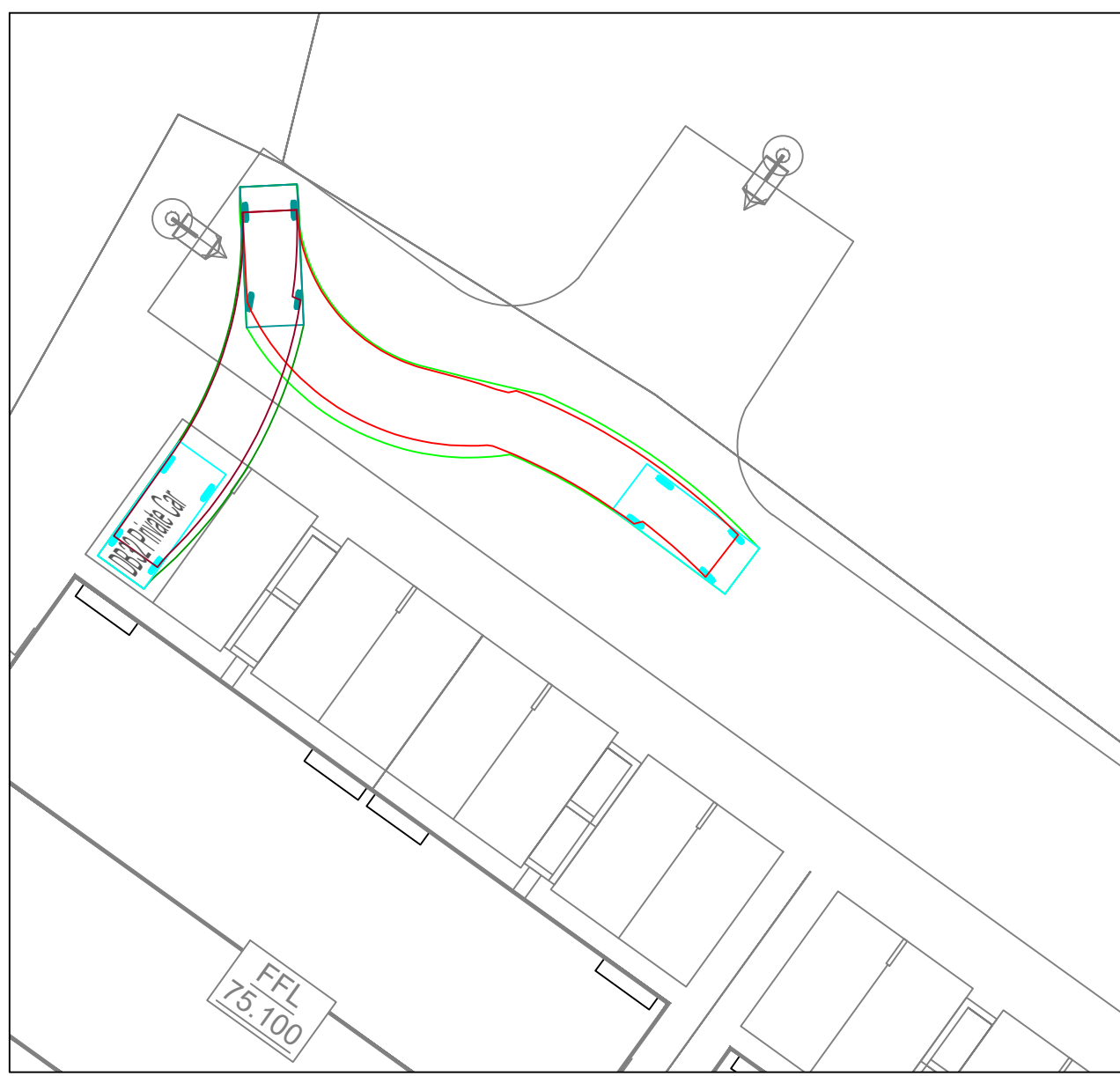
AUTOTRACK STANDARD CAR - FORWARD PARKING **AREA 4**
SCALE: 1:200



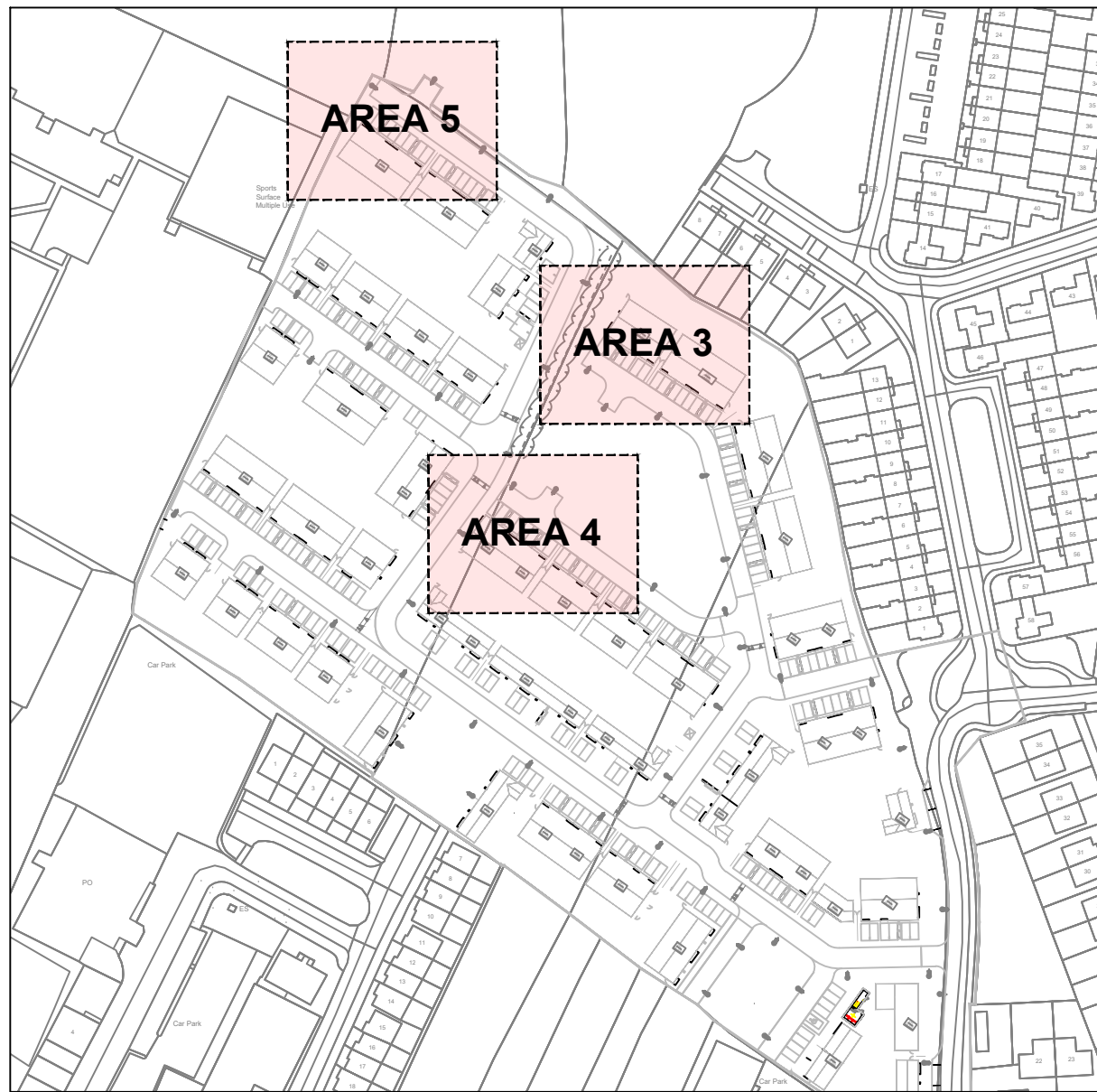
AUTOTRACK STANDARD CAR - REVERSING OUT AREA 4
SCALE: 1:200



AUTOTRACK STANDARD CAR - FORWARD PARKING AREA 5
SCALE: 1:200



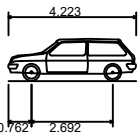
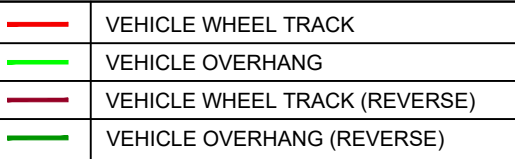
AUTOTRACK STANDARD CAR - REVERSING OUT AREA 5
SCALE: 1:200



KEYPLAN
SCALE; 1:2000

- ## GENERAL NOTES
1. THE CONTRACTOR SHOULD READ THIS ROAD SPECIFICATION IN CONJUNCTION WITH THE RELEVANT TYPICAL DETAILS.
 2. JOINTS BETWEEN NEW ROAD CONSTRUCTION AND EXISTING ROADS SHALL BE AS PER THE DETAILS IN TI-CC-SD-00703. THE EDGES OF THE EXISTING CARRIAGEWAY TO BE CUT BACK BY 0.50m TO FORM A 1.0m WIDE SHOULDER TO FORM A VERTICAL FACE AND FRAMED IN ACCORDANCE WITH TI-CC-SD-00703
ALLOW FOR ALL ADDITIONAL EXCAVATION AND FILLING TO ACHIEVE THE REQUIRED DEPTH OF SUB BASE WHERE NEW AND EXISTING WORKS MEET.
 3. ALL MANHOLES RAISED TO MEET THE NEW ROAD LEVEL WHERE REQUIRED. DISHING CONCRETE TO MANHOLE COVERS AND FRAMES AND INSTALL FOR SETTING FRAME IN CONCRETE TO NEW ROAD LEVELS WHERE REQUIRED PROTECT COVER AND FRAME DURING COURSE OF WORKS. ALL GULLIES TO MEET PROPOSED NEW SURFACE LEVEL WHERE REQUIRED
 4. FOOTPATH EXPANSION JOINTS SHALL BE NEATLY FORMED IN STRAIGHT LINES AT NOT GREATER THAN 3m CENTERS AND SO ARRANGED TO COINCIDE WITH THE JOINTS IN THE KERB JOINTS SHALL BE FORMED BY INSERTING A DOUBLE LAYER OF ROOFING FELT OR OTHER APPROVED METHOD OF ROOFING FELT. THE FULL THICKNESS OF THE SLAB AND BE FINISHED OFF NEATLY AT THE SURFACE. THE CONTRACTOR SHALL ENSURE THE DOUBLE LAYER OF ROOFING FELT IS SUPPORTED IN THE JOINT AND HELD IN A STRAIGHT LINE DURING THE CONSTRUCTION PROCESS.
 5. IN-SITU CONCRETE SHALL BE POURED ON A SUB-BASE OF 150mm NOMINAL THICK OF GRANULAR MATERIAL COMPLYING WITH CLAUSE 808. CONCRETE SHALL BE LAID AND COMPACTED TO THE FINISHED SURFACE TO THE SERIES OF THE SPECIFICATION FOR ROAD WORKS. ALL MATERIALS SPECIFIED SHALL COMPLY WITH REQUIREMENTS OF SR 21 (CONCRETE) AND SR 10 (AGGREGATE) AND BASED ON THE REPORT OF PYRITE (PANEL 212) AGGREGATED FOR USE IN UNBOUND & HYDRAULICALLY BOUND GRANULAR MATERIALS
 6. THE VERTICAL ALIGNMENT OF THE FINISHED SURFACE SHALL NOT DEPART FROM THE DESIGN LEVEL BY MORE THAN +10mm at ANY POINT. THE FINISHED SURFACE SHALL BE A STRAIGHT UNDER A STRAIGHT EDGE SHALL NOT BE GREATER THAN 5mm IN 3m. THE CONTRACTOR SHALL ALLOW FOR THE PROTECTIVE COVER AND FRAME TO BE IN CHAMBERS. MANHOLES AND DUCTING THROUGHOUT THE WORKS ALL CONCRETE JOINTS AND EDGES SHALL BE BULL NOSED.
 7. CBR TESTS SHALL BE CARRIED OUT ON THE SUBGRADE AT FORMATION LEVEL. THE RATE OF THE TESTS SHALL BE 1 TEST PER 50 M² OF SUBGRADE. WHERE THE SUBGRADE TESTS VARY SIGNIFICANTLY ADDITIONAL TESTS MAY BE REQUIRED AT THE DISCRETION OF THE ENGINEER
 8. CAPPING LAYER SPECIFIED BASED ON ESTIMATED SUBGRADE CBR VALUE OF 8%. CAPPING LAYER MAY BE SUBSTITUTED BY ANOTHER SUBJECT TO ACTUAL SUBGRADE CBR TEST VALUES OBTAINED ON SITE.

AUTOTRACK LEGEND



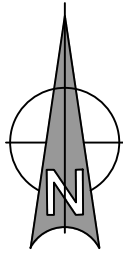
DB32 Private Car	
Overall Length	4.223m
Overall Width	1.715m
Overall Body Height	1.392m
Min Body Ground Clearance	0.233m
Max Track Width	1.629m
Lock-to-lock time	4.00s
Curb to Curb Turning Radius	5.780m

INFORMATION

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Figured dimension only to be taken from this drawing.
All dimensions to be checked on site.
Consultants to be informed immediately of any
discrepancies before work proceeds.

REV NO:	DATE:	REVISION NOTE:	DWN BY:	CKD BY:
P01	21/03/2025	ISSUED FOR COORDINATION	EK	DG
P02	02/04/2025	ISSUED FOR PLANNING	PM	DG
P03	02/05/2025	ISSUED FOR PLANNING	PM	DG



CLIENT:	CORCOM ENTERPRISES LTD.				
PROJECT:	HOUSING DEVELOPMENT AT KINNEGAD, Co.WESTMEATH				
TITLE:	AUTORACK ANALYSIS FOR PRIVATE CAR PARKING AREAS 3, 4 & 5				
DRAWN: PM	CHECKED: DG	APPROVED: MH	JOB NO: 241139		REV: P03
DATE: MARCH 2025	SCALE: 1:200		DRAWING NO: 241139-ORS-ZZ-00-DR-TR-733		

